

1:100 PROPOSED GROUND FLOOR PLAN

PROPOSED CONSTRUCTION MATERIALS:

ROOF:
CLAY PANTILE ROOF COVER.

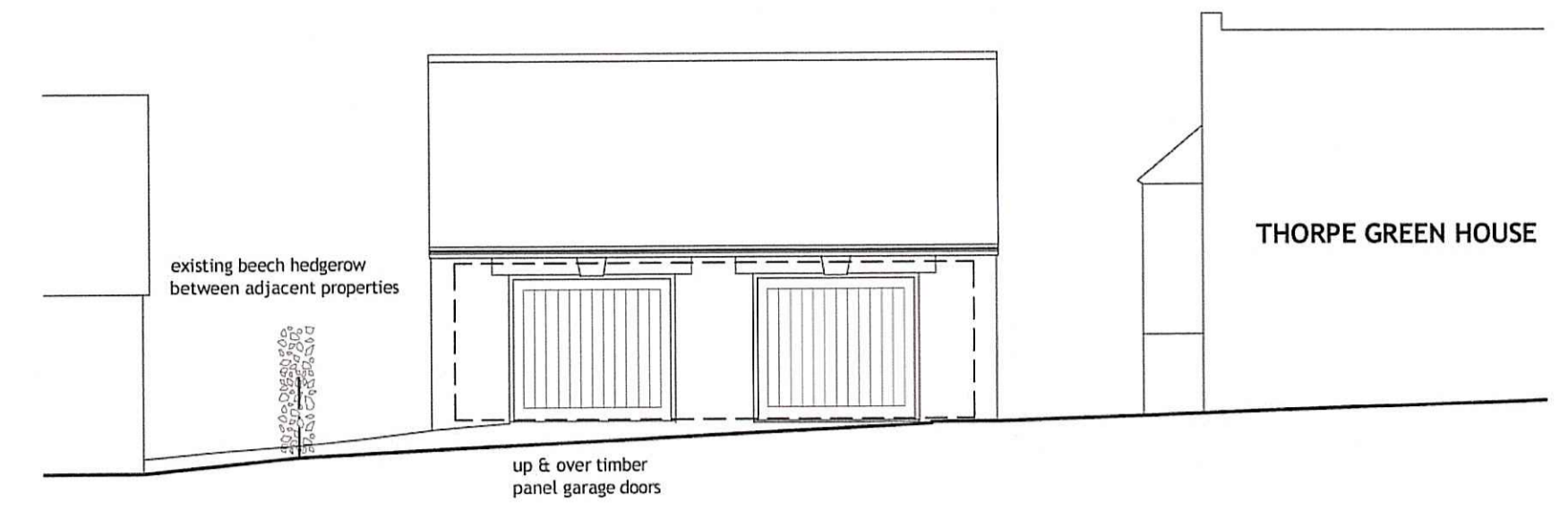
WALLS:
NATURAL STONE, COURSED MASONRY CAVITY WALL CONSTRUCTION TO THE FRONT OF THE GARAGE AND RETURNS OF THE SIDE WALLS. FEATURE KEYSTONE MASONRY LINTOLS TO THE HEAD OF EACH DOOR TO THE DOUBLE GARAGE.

REMAINING SIDE WALLS AND REAR ELEVATION FINISHED IN BUFF COLOURED THROUGH COLOUR RENDER.

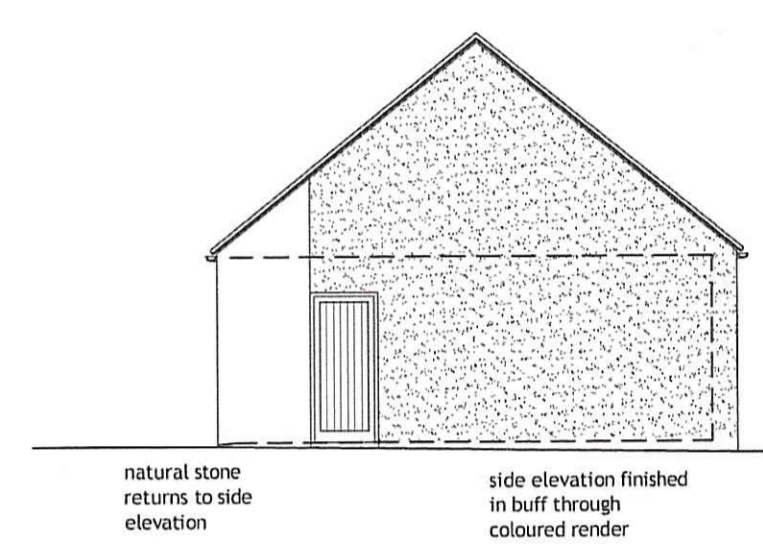
DOORS:
VERTICAL BOARDED PAINTED HARDWOOD DOORS TO MATCH EXISTING RESIDENCE.

WINDOWS:
PAINTED HARDWOOD WINDOWS TO MATCH EXISTING RESIDENCE.

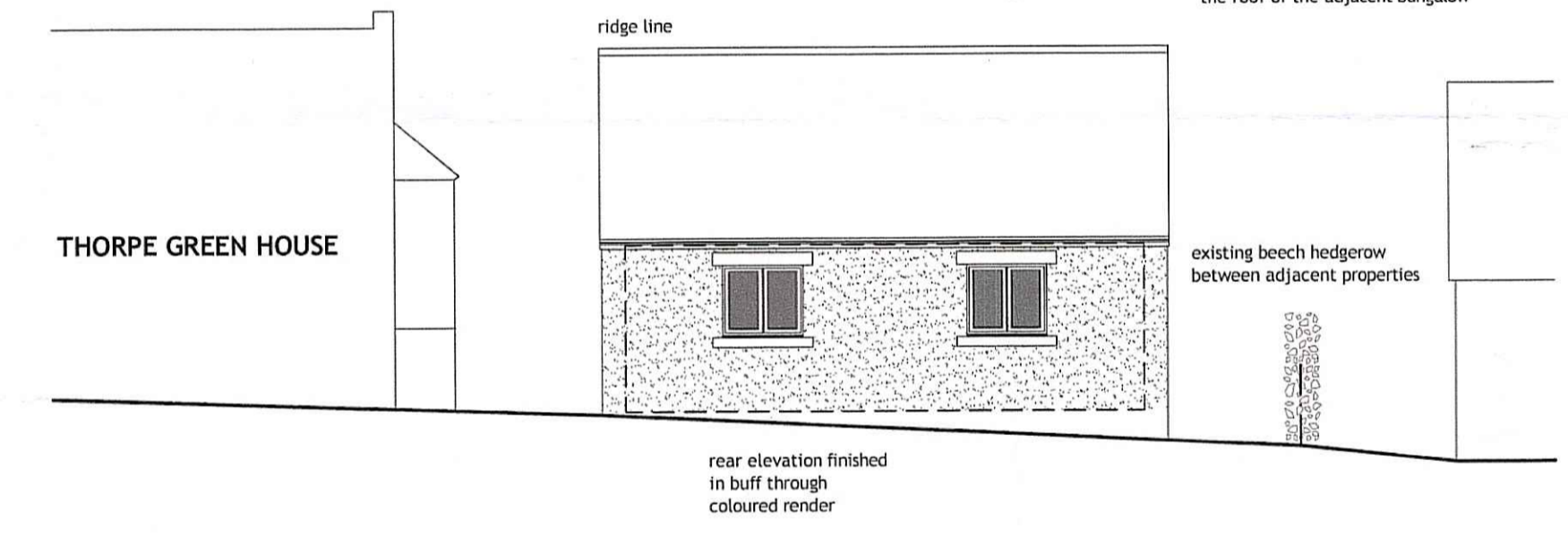
EXTERNAL HARD SURFACES:
NATURAL STONE FLAGS TO DRIVE TO BE RETAINED AND EXTENDED AS SHOWN. POROUS GRAVEL SURFACE WITH TERAM FILTER LAYER BELOW TO BE PROVIDED TO THE FRONT APRON OF THE NEW DOUBLE GARAGE - NOMINAL REMOVAL OF EXISTING TURF REQUIRED TO SET OUT NEW APRON AREA.



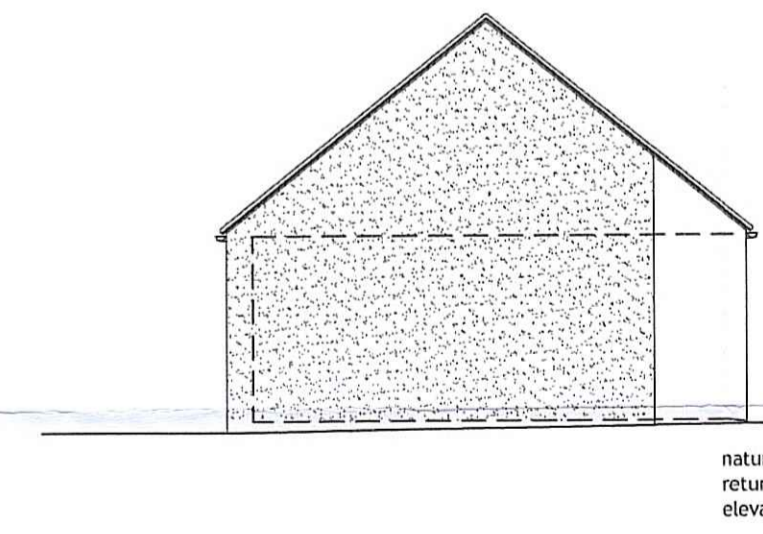
1:100 PROPOSED NORTH ELEVATION



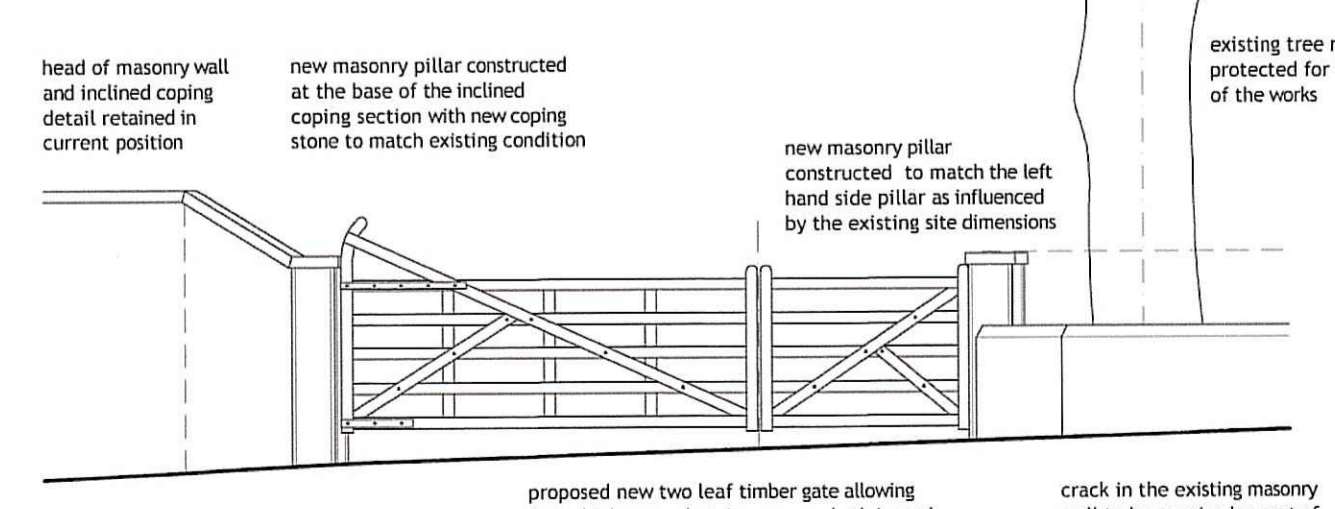
1:100 PROPOSED WEST ELEVATION



1:100 PROPOSED SOUTH ELEVATION



1:100 PROPOSED EAST ELEVATION



1:50 PROPOSED SITE ENTRANCE

AMENDED

Notes
Revised garage position + removal of dormer
RPA for new + Method Statement

NYMNP
25 JUL 2012

WORKS WITHIN ROOT PROTECTION AREA - METHOD STATEMENT:

- The existing tree is 720mm diameter with a root protection area (RPA) with a radius of 6600mm or area of 234m². Due to the rooting zone being restricted by the road and house, the area required by the tree (shown in green hatching) has been applied to the area of the site likely used by it.
 - Works to the rear of the site will be accessed off the existing drive surface to avoid compaction elsewhere on the site. If the external works package includes the lifting and re-laying of the drive surface this then it must be done working from above the existing surface (if any vehicles will be used) and not involve any excavation of the existing ground condition. Flags will need to be re-laid on a suitable load bearing surface (eg sharp sand or sand/geotextile) on top of existing ground levels. The existing flags would be lifted and stored to the rear of the site away from the designated RPA with the new geotextile membrane laid over the existing ground and a nominal 25 to 50mm sharp sand bedding layer provided to even out the surface and provide bedding for the flags to be relaid upon. Once the area is prepared the flags would be laid from the rear of the site out to the threshold with the back edge of pavement.
 - The proposed works include the widening of the drive (to the area shown on plan) by laying flags on the existing ground surface, again, no excavation and supported on a membrane and/or sand to even the ground.
 - The RPA will be protected (fenced using herres panels the line of which is shown in blue) during development of the site when construction of the garage and the gravel drive in front of the garage proceeds to avoid further compaction. Fencelines have been shown on the adjacent tree protection plan as part of this method statement and are to remain in position for the duration of the works.
 - Where the new gravel drive overlaps the RPA it should be constructed according to the advice in BS5837/our Tree and Landscape Design Guide (no dig/suitable load bearing base etc).
- The proposed drive area to the front of the garage within the RPA is to be formed over a geotextile membrane and sharp sand bedding layers onto which a free draining reinforcement system, such as gravel pave or similar will be placed. This containment system requires no ground level excavation and will spread the vehicle loads from the cars into the existing ground structure, it will also retain the gravel surface and prevent wash out and rutting of the completed surface.
- IMPORTANT:**
All works required to form the gravel apron within the designated RPA area and beyond the line of the protective fencing are to be carried out by hand with no supporting vehicles entering that area in order to mitigate any compaction of the ground.
- The protected RPA is isolated from the general construction site and no vehicles or materials may access or be stored in that area other than general movements in and out of the site across the existing drive surface.
 - The new gate post to the west of the site entrance will be set into a hand dug hole to minimise potential root damage within the RPA.
 - The post on the east side will be removed by hand along with its concrete encasement/foundation, the resulting hole will then be carefully enlarged by hand digging a foundation pit sufficient to receive the new masonry post. The excavated hole will be lined with 1200 gauge visqueen membrane to prevent contamination of the RPA by concrete poured in around the post base.
 - The existing masonry wall will have the top three courses rebbed to remove the structural break currently present in the boundary.
- IMPORTANT:**
During the works to the existing gate post on the east side of the drive the applicant should refer to seeking advice (eg from an arboriculturalist or NYMNP) should roots greater than 25mm be present and which cannot be avoided in order to protect the long term health of the existing tree.
- There should be no contamination of roots from concrete used to position either gate post.

REVISION STATUS:
REVISION C - 25 JULY 2012 - JD
Works within the Root Protection Area clarified inline with recommendations received from NYMNP in their e-mail dated 23 July 2012 - drawing updated to include calculated RPA, protective fence line and method statement for working in and around the designated RPA.

REVISION B - 18 JULY 2012 - JD
Proposed double garage relocated and dormer windows removed inline with recommendations received from NYMNP Planning Authority in their letter dated 9 July 2012.

JOURNEYMAN DESIGN

PROJECT THORPE GREEN HOUSE

**CLIENT MR & MRS VERRILL
THORPE GREEN HOUSE
ROBIN HOOD'S BAY
YO22 4TW**

**TITLE PROPOSED INTERNAL FLOOR PLANS
& EXTERNAL ELEVATIONS**

STATUS	DATE	SCALE	REV
PLANNING	02.05.2012	1:100 / 1:50 @ A1	C

DRAWING NO. TG PL-03

THIS DRAWING IS FOR PLANNING PURPOSES ONLY AND NO OTHER USE.
ALL DIMENSIONS SHOWN ARE PROVISIONAL AND ARE SUBJECT TO ON SITE CONFIRMATION AT THE START OF THE CONSTRUCTION STAGE - ANY DISCREPANCIES ARE TO BE REPORTED TO THE EMPLOYER IMMEDIATELY.

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PHOTOGRAPH OF CURRENT SITE ENTRANCE: 26 APRIL 2012